



Report of the Chief Planning Officer

PLANS PANEL CITY CENTRE

Date: 9 December 2010

Subject: ALF COOKE PRINTWORKS, HUNSLET ROAD, LEEDS LS10 1AR

07/07468/FU PLANNING APPLICATION FOR CHANGE OF USE INVOLVING REFURBISHMENT AND PART DEMOLITION OF PRINTING WORKS TO OFFICES AND ERECTION OF 12 OFFICE UNITS IN EIGHT 3 STOREY BLOCKS WITH ANCILLARY CAFE/RESTAURANT, CAR PARKING AND PUBLIC REALM

07/07469/LI LISTED BUILDING APPLICATION FOR CONVERSION TO OFFICES INCLUDING REFURBISHMENT AND PART DEMOLITION OF PRINTING WORKS

APPLICANT

Rushbond Group

DATE VALID

7 January 2008

TARGET DATE

7 April 2008

Electoral Wards Affected:

No Ward Members consulted
(referred to in report)

Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

RECOMMENDATION:

07/07468/FU Defer and delegate to the Chief Planning Officer for approval, subject to the specified conditions (and any others which he might consider appropriate), following completing of a Section 106 Agreement to cover the following matters:

- Contribution to public transport improvements £137 453.20, split equally prior to commencement of Phases 2 & 3
- Travel plan implementation and monitoring fee £7140, split equally prior to commencement of Phases 2 & 3
- Public access throughout the site 7am-7pm Monday to Sunday in accordance with the phasing plan
- Employment and training opportunities for local people.
- Management fee £2400 payable within one month from the date of completion of the s106 agreement

In the circumstances where the Section 106 Agreement has not been completed within 3 months of the resolution to grant planning permission the final determination of the application shall be delegated to the Chief Planning Officer.

07/07469/LI Defer and delegate to the Chief Planning Officer for approval, subject to the specified conditions (and any others which he might consider appropriate).

Conditions for 07/07468/FU

1. Time Limit (3 years)
2. Development in accordance with approved plans
3. Phasing
4. Samples of all external walling and roofing materials.
5. Construction of a sample panel of all external walling materials
6. Samples of all external surfacing materials
7. Detailed 1:20 scale working drawings shall be submitted including cross sections a) all doorways, b) all windows c) eaves and soffit detail and d) the external treatment and materials to any roof top plant rooms e) canopy to south eastern gable of print hall
8. Hard and/or soft landscaping scheme
9. Implementation of landscaping
10. Maintenance of landscaping scheme
11. Waste storage and disposal details, including recycling and details of security of and access to the bins.
12. No refuse containers to be stored outside the specified areas.
13. Details of installation and operation of air conditioning.
14. Details of a noise attenuation scheme
15. Contaminated land information
16. Amendment of remediation statement
17. Submission of verification reports
18. Oil interceptor
19. No building or other obstruction within 3 metres either side of a water main.
20. Details of works for dealing with surface water discharges from the development required.
21. No piped discharge of surface water from the development prior to completion of approved surface water drainage works.
22. Dust suppression measures during construction.
23. Means of preventing mud on the highway
24. Details of motorcycle parking
25. Details of long and short stay bicycle parking
26. Submission of detailed scheme comprising (i) a recycled material content plan (using the Waste and Resources Programme's (WRAP) recycled content toolkit), (ii) a Site Waste Management Plan for the construction stage, (iii) a waste management plan for the buildings occupation and (iv) a BREEAM assessment to at least Very Good rating or equivalent
27. Programme of archaeological recording required
28. Details of public art
29. Implementation of off-site highways works
30. Maximum car parking provision (Highways Agency)
31. Car parking space designation (Highways Agency)
32. Implementation of flood risk assessment measures
33. Flooding emergency egress and evacuation
34. Tree pits
35. Details of gates and accesses
36. Areas to be used by vehicles to be laid out

37. Provision of shower facilities/lockers in Phases 2 & 3

The following are non standard conditions which can be found in full in the Appendix –

3, 7, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37

Conditions for 07/07469/LI

1. Time Limit (3 years)
2. Development in accordance with approved plans
3. Phasing
4. Samples of all external walling and roofing materials.
5. Construction of a sample panel of all external walling materials
6. Samples of all external surfacing materials
7. Detailed 1:20 scale working drawings shall be submitted including cross sections
1) all doorways, 2) all windows 3) eaves and soffit detail and 4) the external treatment and materials to any roof top plant rooms 5) canopy to south eastern gable of print hall
8. Hard and/or soft landscaping scheme
9. Implementation of landscaping
10. Maintenance of landscaping scheme
11. Programme of archaeological recording
12. Making good to match existing
13. Guttering/downpipe details
14. Building lighting
15. Details of repairs to brickwork and repointing
16. Details of any replacement windows – listed building
17. Junction of new raised ground floor and base of internal columns
18. Building cleaning
19. Details of new handrails to gallery balustrades
20. Tree pits

The following are non standard conditions which can be found in full in the Appendix –

3, 7, 17, 19, 20

Reason for approval 07/07468/FU:

The application is considered to comply with the policies GP5 GP7 BD6 A1 A4 N12 N13 N14 N15 N17 N25 N29 BD4 E14 ARC6 CC1 CC3 CC10 CC11 CC12 CC13 CC19 CC30 T2 T2D T5 T6 T7A LD1 of the UDP Review 2006, the Yorkshire and Humber Regional Spatial Strategy 2008, as well as supplementary planning guidance in SPD5 Public Transport Improvements and Developer Contributions, SPD Travel Plans, and the City Centre Urban Design Strategy, and national guidance contained within PPS1, PPS4, PPS5, PPG13, PPS25 and having regard to all other material considerations, as such the application is recommended for approval.

Reasons for approval 07/07469/LI:

The application is considered to comply with policies GP5 BD6 N12 N13 N14 N17 N15 ARC6 N29, the Yorkshire and Humber Regional Spatial Strategy 2008, and national guidance contained within PPS5 and having regard to all other material considerations, as such the application is recommended for approval.

1.0 INTRODUCTION:

- 1.1 This application is brought to Panel as it is a significant major application involving the conversion and partial demolition of a Grade II listed building.

2.0 PROPOSAL:

- 2.1 The application proposal is for the retention of the two main print halls and the mill building, with conversion of these elements to offices, along with eight three storey office buildings, divided into 12 units, and 114 car parking spaces (including 15 disabled spaces). Total B1 office floorspace proposed would be 16138 square metres.

- 2.2 The development would be constructed over 3 phases:

Phase 1 would consist of:

- Retention of main print halls and ancillary offices, building 3, mill building and late Victorian single storey dispatch area – to be made weather-tight and secure – details of this to be provided by condition.
- Refurbishment of Buildings 3, 4 & 5 for ongoing commercial/industrial use (B2/B8)
- Demolition of Building 6 (late 20th Century warehouse)
- Demolition of Building 2(b) (1920s/1950s north-lit warehouse)

Phase 2 would consist of:

- New build blocks A, C, D and G including laying out of permanent landscaping to half of site, including main green square measuring in total 65m x 34m (with 50m x 20m raised grass area), and laying out of temporary green landscaping to plot of Building E.
- Laying out of 55 car parking spaces
- Building 1 (print halls and mill building to be retained in light industrial/warehouse use with ancillary offices)

Phase 3 would consist of:

- Refurbishment of the mill building and print halls include raising the floor level to accommodate the requirements of the flood risk assessment.
- Victorian dispatch areas 2(a), Building 4 and 1950s print hall extension to Hunslet Road would be demolished
- Erection of Buildings E, H, I, J1, J2, K1, K2 and L including all car parking and landscaping, and improvements to Neal Place.
- Laying out of remainder of 59 car parking spaces.

The retained print halls and mill building would be subject to the following repairs and alterations as part of refurbishment works:

- Phase 1 essential repairs to Print Halls including re-pointing to walls, removal of paintwork, damp proofing, cleaning, new rainwater goods, glazing.
- Phase 3 raised floor level to comply with flood risk assessment to raise floor from 25.45m AOD to 25.70m AOD, and addition of upstand to perimeter of buildings to protect to flood level of 26.15m AOD.
- removal of office partition layout
- insertion of new internal partition arrangement
- new canopy to flank facing Building L
- glazed curtain walling to ground floor frontage facing grassed square
- retention of clock and retention of water tower as focal points

The focal point of the development would be the retained print halls and mill building. It is proposed to use this building as the back drop to a green landscaped pedestrian space. This space would be enclosed by a series of modern 3 storey office buildings, with pedestrian linkages across the site. Car parking would be located to the rear of the mill building within hard landscaped space. Vehicular access would be via the existing access off Hunslet Lane, and via a relocated access off Leathley Road. A further row of 3 storey office buildings would be located behind, with further car parking set within landscaping. Pedestrian access through the site is detailed with a large greened hard and soft landscaped area being provided between the existing building and blocks D, G, H and I.

Some of the new buildings would be constructed in red brick, some in a blue brick, with deep window reveals and glazed curtain walling to stair cores. Each block has a slight variance to its window pattern. Building C features a glazed roof detail replicating a north-lit shed, echoing current buildings on the site. The elevational rhythm of the new blocks sited closest (K1 K2 and L) to the retained buildings would feature an interpretation of the original window patterns.

1:50 and 1:20 typical bays and sections have been submitted for each building to establish the detailing of the elevations

Surfacing consists of a variety of materials, with granite setts immediately around the listed building, block paving to the vehicular routes, conservation tegula paving to main footways, and raised grassed areas. Car parking spaces would be tarmac, demarcated by block paving setts.

Soft landscaping would consist of Hornbeam trees planted at regular intervals throughout the site, with clipped box hedging and raised grass areas edged with granite facings.

The history of the building would be reflected in public art throughout the site, based upon a folded paper motif, including a lightweight steel structure being sited close to the Hunslet Road frontage.

A number of documents have been submitted in support of this proposal:

- Planning Statement
- Heritage Statement
- Supplementary Heritage (PPS5) Statement
- Design and Access Statement
- Sustainability Statement
- Transport Assessment
- Flood Risk Assessment
- PPS25 Sequential Test Assessment
- Drainage Statement
- Noise Survey
- Ventilation Strategy
- Phase 1 Land Contamination Report
- Travel Plan
- Archaeological Assessment

3.0 SITE AND SURROUNDINGS:

3.1 The 1.95 hectare application site is unallocated within the designated City Centre. It lies on the south western side of Hunslet Road, at its junction with Leathley Road.

The site is bounded to the south west by an overgrown public footpath Neal Place, beyond which is the Scala Court business park, consisting of single storey red-brick warehouse units. The southern boundary is marked by a 4m high detailed engineering brick wall to Leathley Road, facing the Costco wholesale warehouse to the south of the site. To the north lies the cleared former Yorkshire Chemicals site, and beyond that, the Tetley Brewery site. Part of this area to the north lies within the draft South Bank Planning Statement and Aire Valley Urban Eco-Settlement proposals. To the east lies a car showroom, and beyond that lies Clarence Dock and the River Aire.

- 3.2 The Grade II Listed printworks complex is currently vacant, and consists of a principal 3 storey red-brick building with terracotta and stone dressings with a slate roof. The corner of the building facing the junction of Black Bull Street and Hunslet Road features a clock tower, which is an important local landmark and visual focal point within Leeds City Centre. There is Yorkstone paving to the Hunslet Road frontage. Surfacing to the loading yard is a mixture of tarmac and concrete surfaces.
- 3.3 The main print hall Hunslet Road elevation features repetitive alternate bays of segmental headed windows and projecting bays with pedimented gable canopies over small semi-circular headed windows at ground floor. At first floor the windows feature elliptical arches alternating in stone and brick, and at second floor two-light headed windows with columns in between. The other elevations of the main print hall building retain the same proportions but are less ornate.
- 3.4 The interior of the two main print halls are open, with spiral staircases leading up to cast iron-balustraded landings around each space. Cast iron columns support these mezzanine floors, which are connected by the spiral staircases. Immediately behind the main print halls is a red-brick four storey mill building, which is much less ornate than the main print halls. This features a water tower and chimney dating from circa 1894. Adjacent to the Mill Building is a single storey dispatch area, of a similar age. This consists of the first 4 bays of a double pitched roofed building, with symmetrical timber roof trusses supported by round cast iron columns. The outer walls of the dispatch area were demolished to form later additions.
- 3.5 Later Twentieth Century additions include a c1920s large north-lit warehouse structure supported on steel I-beams, extending out to the south from the dispatch area, a 1950s concrete extension to the Hunslet Road frontage, and along the boundary to Leathley Road, and more recent late 20th Century warehouse buildings along the western boundary.

4.0 RELEVANT PLANNING HISTORY:

- 4.1 None

5.0 HISTORY OF NEGOTIATIONS:

- 5.1 Pre-application discussions have been on-going since early 2007. Pre-application presentation to Members by the developer and architect at Plans Panel (City Centre) 16 August 2007 and the following comments minuted, and subsequently agreed on 13 September 2007 :
- (a) the respectful and complimentary nature of the scheme;
 - (b) whether wider pavements could be created on Hunslet Road to encourage pedestrians along this route into the City Centre;

- (c) the need for pedestrian connectivity around the site and the possibility of creating links into the wider area
- (d) how blocks A to L would work and the need for officers to consider the layout and massing, particularly blocks D to G and H and I which mirrored the length of the existing building; concern at some elements of the replication of the existing mill building fenestration;
- (e) that the new buildings should possibly be more ornate and include details such as copings or parapets;
- (f) that a site visit should be made the next time the proposals are considered.

5.2 Full planning and listed building consent applications were submitted in November 2007 and validated January 2008. Further information was requested by officers on a number of matters in March 2008. Application progress was held up by a Highways Agency Article 14 Direction on 1 May 2008 thus preventing the Local Authority from making a decision, and subsequently lifted on 23 March 2009. Discussions with officers, and subsequent revisions to the scheme continued to evolve regarding off-site highways works, public transport contribution, layout, detailed new building design, landscaping, internal layout of listed building conversion and phasing of demolition until November 2010.

6.0 PUBLIC/LOCAL RESPONSE:

6.1 Application publicity consisted of:

- (a) Site Notice 07/07468/FU Notice of Proposed Major Development posted 11 January 2008, expired 1 February 2008.
- (b) Site Notice 07/07468/FU Notice of Proposed Major Development posted 16 April 2010, expired 7 May 2010
- (c) Site Notice 07/07469/LI Notice of application for Listed Building Consent posted 11 January 2008, expired 1 February 2008.
- (d) Site Notice 07/07469/LI Notice of application for Listed Building Consent posted 16 April 2010, expired 7 May 2010.
- (e) Press Notice 07/07468/FU Notice of Major Development published 17 January 2008, expired 7 February 2008.
- (f) Press Notice 07/07468/FU Notice of Major Development published 29 April 2010, expired 20 May 2010
- (g) Press Notice 07/07469/LI Notice of application for Listed Building Consent published 17 January 2008, expired 7 February 2008.
- (h) Press Notice 07/07469/LI Notice of application for Listed Building Consent published 29 April 2010, expired 20 May 2010

6.2 Letter from Leeds Civic Trust, dated 18 March 2008.

Expression of support to the principles of the development, and the creation of a significant soft 'public' green space. However, they had concerns regarding:

- (a) the new brick buildings, which they considered were austere and flat, and needed to be finished to a far higher standard than most new build brick buildings in the City. Choice of brick very important; *Response: The scheme has been revised, to include variety in brickwork, depth of window reveal, and changes in window patterning. Quality of detailing would be conditioned through detailed sections and material sample panels including mortar details.*
- (b) concern that small windows with deep reveals may increase the requirement for artificial lighting in the new build offices, which would not be in keeping with the developers stated sustainability aims; *Response: The scheme has been revised and the amount of glazing on the proposed new buildings has increased.*

- (c) there is potential for greater variety in the finishes in the new buildings, in order to lighten the appearance of the scheme, and mask imperfections in build quality; *Response: Scheme has been revised, there is now variety in treatment of buildings in design, window patterns and brick type.*
- (d) improvements to pedestrian connectivity in the area, particularly to the east, for example wide pedestrian crossings, more direct routes for pedestrians, longer pedestrian phases at lights, etc. *Response: There is a package of pedestrian improvements (set out in the Highways section of the Appraisal in this report) to be completed by the final phase of the development, including new pedestrian crossing facilities. Nearby planning permission 06/04601/OT at former Yorkshire Chemicals site, Black Bull Street also proposes improvements to achieve better pedestrian connectivity in the Hunslet Road area including upgrade of existing crossing at Hunslet Lane close to Butterley Street, 2 signalised crossings on Black Bull Street, zebra crossing on Chadwick Street, signalised crossing facilities at junction of Hunslet Lane and Crown Point Road.*

6.3 Letter from Leeds Civic Trust dated 9 October 2008.
Expression of support – concern regarding public art being sited along Hunslet Road, obscuring detailing of the building; and suggestions regarding highways network in Hunslet Road area namely removing roadspace, removing “high-speed turnouts”, and simplifying routes for pedestrians and cycles. *Response: Public art would be subject to details to be submitted by condition. In principle it is considered that appropriate public art, inspired by the previous use of the building, would be acceptable and would enhance the setting of the listed building. It is considered that the application proposal, subject to improvements set out in the Highways section of the Appraisal, would be accessed satisfactorily on foot, cycle, motorcycle, private car and public transport.*

6.4 Letter from Leeds Civic Trust dated 15 December 2009.
Expression of support – Leeds Civic Trust acknowledge the need for continuing temporary uses at the site, and welcome the delivery of the open grassed courtyard in an early phase or a temporary landscaping scheme until development comes forward. They also welcome the revisions to the scheme including reduction of internal works, and restoration of detailing to print hall balconies, and rainwater downpipes. *Response: The grassed courtyard would be delivered in Phase 2, along with temporary landscaping to the plot of Building E (to be delivered in Phase 3). The restoration of detailing to print hall balconies, and details of rainwater goods, would be conditioned.*

7.0 CONSULTATIONS RESPONSES:

7.1 Statutory:

- (a) English Heritage (dated 15 November 2010) - no comment on this particular case, the application should be determined in accordance with national and local policy on the basis of local specialist advice. Leeds City Council Conservation team advised no objection subject to specified conditions regarding details of rainwater goods, lighting, treatment of surfaces following demolition/temporary landscaping; details of repairs to brickwork and repointing; and details of new handrails to gallery balustrades. Assessment of conservation issues is covered in the appraisal section of this report.

- (b) Highways Agency: No objection subject to specified conditions and travel plan measures.
- (c) Leeds City Council Highways Development Services: no objection subject to implementation of travel plan, provision of contribution to public transport infrastructure, improvements to local bus stops, cycle storage provision, motorcycle storage provision, off-site highways works including improvements to Hunslet Road and Leathley Road accesses and improved pedestrian crossing facilities at junction of Hunslet Road and Leathley Road.
- (d) Environment Agency: no objections subject to specified conditions.
- (e) Yorkshire Water: no objections subject to specified conditions.

7.2 Non-statutory:

- (a) West Yorkshire Archaeology Advisory Service (responses dated 1 February 2008; 29 October 2010; 23 November 2010) – no objection, the demolition of later additions can be justified on the grounds that those buildings are of lesser historical and architectural significance, although arguably they are contemporary with the main multi-storied buildings. A condition should be added to secure the implementation of an appropriate programme of archaeological investigation before works commence.
- (b) Leeds City Council Environmental Protection: No objection subject to conditions regarding mechanical plant installation.
- (c) Leeds City Council Land Drainage: No objection subject to conditions regarding permeable surfacing.
- (d) Leeds City Council Public Rights of Way: Neal Place may be considered to be a Public Right of Way, however the site lies within an area of Leeds unrecorded on the Definitive Map.
- (e) Victorian Society – Supports the revised scheme which proposes less intervention in the print halls, and would be more appropriate to the retained listed building.

8.0 PLANNING POLICIES:

8.1 Development Plan

Regional Spatial Strategy Yorkshire and the Humber 2008

Leeds Unitary Development Plan Review 2006

Relevant policies include:

GP5 all relevant planning considerations

GP7 planning obligations

BD6 all alterations

A1 improving access for all

A4 safety and security provision

N12 urban design

N13 design and new buildings

N14 listed buildings and preservation

N15 listed buildings and change of use

N17 listed buildings character and appearance
N25 boundary treatments
N29 archaeology
ARC6 archaeology
BD4 all mechanical plant
E14 office use in the City Centre
CC1 City Centre and planning obligations
CC3 City Centre character
CC10 public space and level of provision
CC11 streets and pedestrian corridors
CC12 public space and connectivity
CC13 public spaces and design criteria
CC19 office development
CC30 unallocated sites within the City Centre
T2 transport provision for development
T2D public transport provision for development
T5 pedestrian and cycle provision
T6 provision for the disabled
T7A cycle parking
LD1 landscaping

8.2 Relevant Supplementary Planning Guidance includes:
SPD5 Public Transport Improvements and Developer Contributions
SPD Travel Plans
City Centre Urban Design Strategy

8.3 National planning policy and guidance includes:
PPS1 Delivering Sustainable Development
PPS4 Planning for Sustainable Economic Growth
PPS5 Planning and the Historic Environment
PPG13 Transport
PPS25 Development and Flood Risk

9.0 MAIN ISSUES

1. Principle of use
2. Urban design
3. Impact of the proposal on the special character of the listed building
4. Highways
5. Flood risk
6. Sustainability
7. Section 106 Agreement

10.0 APPRAISAL

10.1 Principle of use

The application site lies within the designated City Centre, but is unallocated for any particular uses. Therefore UDP Policy CC30 states that proposals of this nature would be determined on their merits. Office use would therefore be acceptable in principle as identified by national policy in the form of PPS4 which seeks to concentrate office uses in town or city centres, and promote sustainable economic growth and employment. The proposal is also acceptable in principle under UDP Policies CC19 and E14, which seek to ensure that the City Centre is the principal location for prime office development. It is not considered that the proposed use would give rise to amenity concerns, and the office accommodation would be

subject to a condition to ensure noise from external sources does not cause nuisance to future employees. In terms of supporting uses to serve the office users, the site is located close to Crown Point Retail Park and Clarence Dock. There is also supporting retail/restaurant/bar/leisure/community facilities in planning permission 06/04601/OT former Yorkshire Chemicals site should that site be developed in the same timescale. It is therefore considered that the provision of a mix of uses is not necessary in this case.

10.2 **Impact of the proposal on the special character and interest of the listed building.**

In accordance with national policy PPS5, and Leeds UDP Review policies, there is a presumption in favour of the preservation of listed buildings. The submitted Heritage Assessment, and Supplementary Heritage Assessment, put forward an assessment of the nature, extent and importance of the significance of the heritage assets at the site. Detailed justification for demolition of various elements has been submitted in accordance with PPS5, setting out the nature of the interest and the significance of the interest, which has been assessed and agreed by the Council's specialist conservation and archaeology advisors.

The scheme results in a phased retention of buildings, with those of most merit and capable of viable conversion and enhancement being retained. The remainder of the site would be demolished in phases. In terms of assessing significance, the main print hall building is listed due to its special architectural interest, and would be retained and converted into offices, with its open atria and balconies restored. The original proposals included the infilling of the atria with glazed office pods, however this has now been removed from the scheme. The Mill Building would also be retained and refurbished as office accommodation, but is of secondary significance to the main print halls.

The single storey dispatch sheds 2(a) as labelled on the plans are of some importance, but lesser than the print halls and mill building, as they reflect the expansion of the works not long after its construction, their southern and eastern walls removed for later extension. At the greatest extent on 2(a) they consist of the first 4 bays of a double pitched roofed building, with symmetrical timber roof trusses supported by round cast iron columns. This was subsequently extended post-1920 with a north-lit warehouse structure supported on steel I-beam columns, identified on plan as building 2(b). It is considered that the demolition of the dispatch sheds is justified due to their lesser significance both historically and architecturally, their lack of options for viable conversion, poor state of repair, and because their removal would allow the delivery of public open space to the main entrance of the refurbished building, which would enhance its setting. The demolition also allows the coherent urban design of the site, and the connection between Building L and the print halls.

It is considered that the loss of the later 20th Century additions is acceptable. The Hunslet Road extension (Building 3), and buildings 2(b), 4, 5, and 6 are of little architectural and historical interest, and their demolition would enable the enhancements of the eventual comprehensive new build and landscaping scheme, the benefits of which would outweigh their loss. It is therefore considered that the proposed phased demolition would enhance the special character of the listed building.

In terms of the internal alterations to the print halls and the mill building, where possible, existing detailing and all features including internal features which contribute to the character of the listed building would be preserved and repaired.

The original plan form of the building would be preserved since it contributes to the special character and appearance of the building. It is considered that the original use of the listed building is unfortunately no longer viable (manufacturing), therefore proposals for its change of use and refurbishment are viewed favourably, given that the new and adapted use would not diminish the special architectural or historic value of the building and its setting.

10.3 **Urban design principles and the impact of the new buildings and public realm on the setting of the listed building.**

The objective of the proposed new buildings is to provide calm background buildings which would support and not compete visually with the prominent character of the listed print works. However they would provide quality and visual interest in terms of their form, rhythm, materials, and appropriate modern detailing.

It is considered that high quality red brickwork and blue brickwork would be appropriate materials given the setting of the listed building, the quality of the brickwork can be controlled through material samples panels including mortar joints by condition. 1:50 and 1:20 typical bays and sections have been submitted for each building to establish the detailing of the elevations, and this would be reinforced at working drawing stage by conditions. All mechanical plant would be concealed within the envelope of the building, full details would be required by condition.

The useable public realm on the site would be approximately 20% of its site area. This would include the main grassed square, and the hard landscaped margins, plus the smaller green areas to Hunslet Road and adjacent to Buildings D & G. The site would be maintained as publicly accessible private land from Phase 2 via the Section 106 agreement from 7am-7pm, in order to tie in with office hours, to ensure adequate security and protection of the space outside office hours in this predominantly industrial/commercial area.

The proposed public space would form part of a potential new network of public spaces, linking together the Brewery site, the proposals for the former Yorkshire Chemicals site, and Clarence Dock. The site provides enhanced connectivity across the area, and the improvement and connection to Neal Place. This gives a new connection through the site, albeit linking into a stable commercial area where at present there are no proposals for new developments or new connections, however this may not always be the case in future.

The boundaries of the site would be designed in a positive manner appropriate to the character of the area and the character of paving materials would accord with the character of the listed building, with the use of high quality natural paving immediately around it. Exact details of hard and soft landscaping, including samples of surfacing materials, and details of achieving level access onto the main grassed area would be controlled by condition prior to commencement of each phase of development to ensure continuity of high quality materials across the site.

The pedestrianised spaces within the development would be attractive and enhance the setting of the listed building, and support daytime activity, in particular lunchtime use. It is considered that the scheme would retain and reinforce the identity and distinctive character of this building, city centre, and would upgrade physical environment to complement needs of activities essential to identity vitality and function of the city centre. The proposal would upgrade the physical environment and to create a place that is sympathetic to the historic character of the listed

building, complementary in scale and materials, with a modern contrast in detailing.

Alf Cookes Printworks is a very important building in terms of local identity, distinctiveness and legibility. It is one of Leeds best known buildings of the past, and this scheme would enable public access and appreciation alongside new buildings and public realm

It is therefore considered that the proposed new buildings and public realm would enhance and complement the setting of the listed building and the surrounding area.

10.4 **Highways**

The site lies within the Fringe parking area, and the 114 space parking provision proposed is under the UDP maximum parking for the site. However the site lies within the City Centre in an accessible location, well served by public transport, and supported by travel plan measures.

Measures provided at planning application stage :

- Travel Plan sets out information on measures and relevant contacts for promotion of more sustainable travel patterns including accessibility by walking, cycling, motorcycles, public transport, car club, work-based pool cars, private car share.
- Shower facilities and lockers in each block
- Appropriate secure storage for cyclists and motorcyclists
- Improvements to local pedestrian connectivity through improved crossing facilities on Hunslet Lane and improvements to Neal Place
- Improvements to two bus stops on Hunslet Lane including shelters with real-time display
- Strategic public transport contribution

Ongoing Travel Plan measures to be monitored via the Section 106 Agreement from start of Phase 2 include:

- Appointment of a travel plan coordinator for each business on the site within 1 month of occupation
- Provision of travel to work survey for each unit within 3 months of occupation
- Setting of targets for businesses for modal split and specific initiatives set out in order to meet those targets
- Submission of individual travel plans including setting out the above matters in accordance with the Travel Plans SPD within 6 months of occupation
- Review of Travel Plan annually
- Overall target of reducing single occupancy car journeys to 30% of total morning and evening peak journeys to work.

The applicant is to provide new pedestrian crossing facilities on Hunslet Lane, to facilitate better connections to Clarence Dock and the riverside, and would re-open and improve the pedestrian route along Neal Place, to enable future connectivity to the southeast/southwest.

In order to achieve safe access to the site, conditions regarding reconfiguration of the accesses to Hunslet Road, prior to occupation of Phase 2 and Leathley Road, prior to occupation of Phase 3, would be applied.

It is considered that the proposed development would be served adequately by existing highways or improvements to the highways network which are funded by the developer via planning conditions or planning obligations, and would not create or materially add to problems of safety, environment or efficiency on the highway

network. The development would be adequately served by public transport and would provide for enhancements to the strategic public transport network, improve two local bus stops on Hunslet Lane, as well as provide improvements to local pedestrian routes where required to achieve appropriate levels of accessibility. The development would also provide and encourage safe and secure cycle and motorcycle usage/storage, and an adequate level of car parking.

10.5 **Flood Risk**

The Local Planning Authority is satisfied that the Sequential Test for the above planning application has been undertaken in a transparent and open way, in full accordance with Planning Policy Statement 25 and its Practice Guide, and has been passed. It is considered that as the site is a listed building, the scheme cannot be replicated elsewhere in a lower risk zone. The proposed development is for a use which is classed as 'less vulnerable' under PPS25 and is therefore appropriate in Flood Zone 3A. The scheme will result in a reduction in the built area and provide more space for storage of flood water. It is an appropriate use for the City Centre, and one which under PPS4 should not be located outside a designated centre, and one which would secure the viable use of a vacant listed building.

10.6 **Sustainability**

New buildings should be designed to maximise opportunities to conserve energy and water resources and use materials appropriate to these aims, and should ensure satisfactory penetration of daylight and sunlight. The new build elements will meet BREEAM Very Good rating, and a planning condition to provide details of the verification of this will be applied. In terms of sustainable drainage, permeable surfacing will be required by condition. In relation to the concern of Leeds Civic Trust that deep window reveals may compromise the sustainability credentials of the development, it is considered that shallow reveals may lead to a poorer appearance, and lessen the quality of the design.

10.7 **Section 106 Agreement**

A Section 106 Agreement would be signed in connection with the planning application, with the following obligations:

- (a) public realm delivery and access through the site in accordance with the relevant phasing plan, would be secured via the Section 106 agreement;
- (c) a public transport contribution of £137 453.20 is required by SPD5 taking into account discounts for existing trip levels, other infrastructure provisions and that the development involves the retention and refurbishment of a listed building. This would be split with £68 726.60 payable prior to commencement of Phase 2, and £68 726.60 payable prior to commencement of Phase 3 ;
- (d) a travel plan monitoring fee of £7140 would be required by Section 106 agreement, split £3570 prior to commencement of Phase 2, £3570 prior to commencement of Phase 3.
- (e) cooperation with local jobs and skills training initiatives would be expected through the provisions of a Section 106 clause.

As part of Central Government's move to streamlining the planning obligation process it has introduced the Community Infrastructure Levy Regulations 2010. This came in to force on 6 April 2010 and requires that all matters to be resolved by a Section 106 planning obligation have to pass 3 statutory tests. The relevant tests are set out in regulation 122 of the Regulations and are as follows:

'122(2) A planning obligation may only constitute a reason for granting planning permission for the development if the obligation is-

- (a) necessary to make the development acceptable in planning terms;
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

As listed above (and also in the 'recommendation' box at the beginning of this report), there are matters to be covered by a Section 106 agreement. These matters have been considered against the current tests and are considered necessary, directly related to the development and fairly and reasonably related in scale and kind to the development.

11.0 CONCLUSION

- 11.1 It is considered that the submitted listed building consent and full planning applications would result in the re-use of a brownfield site, the provision of new employment in the City Centre, the concentration of a major town centre use in a sustainable location, the sensitive re-use of a listed building and the provision of new public open space. It is therefore considered that the proposal would contribute positively to the enhancement and regeneration of the surrounding area.

Background Papers:

Application files 07/07468/FU & 07/07469/LI

Certificate of Ownership A signed by applicants

Appendix 1

07/07468/FU

- 3) Unless otherwise agreed in writing by the Local Planning Authority, no works of demolition, or alteration to the interior or exterior of the building, shall take place other than in complete accordance with a programme of demolition and redevelopment identified on Seven Architecture drawing refs. 060127-111 Revision E, 060127-12 Revision E, and 060127-113 Revision D.
- (a) In phase 1, prior to the demolition of Building 2b, details for the temporary treatment of the exposed elevation to Building 2a shall be submitted to and approved in writing by the Local Planning Authority, and implemented upon completion of demolition, unless otherwise agreed in writing.
 - (b) In phase 2, no demolition works to Building 2b shall take place until a contract for the implementation of the new public square and the works to the ground floor treatment of Building 1 shown on Seven Architecture Drawing Ref. 060127-232. These works shall be completed within 3 months of first occupation of the first new office building.
 - (c) Details for each relevant condition below shall then be submitted in accordance with the phasing schedule unless otherwise agreed in writing by the Local Planning Authority. Any subsequent changes to the phasing schedule shall be submitted in writing to and approved by the Local Planning Authority. The scheme shall be built out in accordance with the approved phasing plan.

In order to accord with the provisions of the Leeds Unitary Development Plan Review 2006, in the interests of visual amenity, the special character of the listed building, pedestrian connectivity, highways safety, sustainable development, and in order that the Local Planning Authority is informed of the phasing in order that the relevant sections of the conditions may be discharged.

- 7) No building operations shall take place within a phase until typical 1:20/1:50 details of the following have been submitted to and approved in writing by the Local Planning Authority:
- a) all doorways
 - b) all windows
 - c) eaves and soffit detail
 - d) the external treatment and materials to any roof top plant rooms
 - e) canopy to south eastern gable of print hall

Works shall be carried out in accordance with the details thereby approved, and retained as such thereafter.

In the interests of visual amenity, design quality and the setting of the listed building.

- 28) Prior to the commencement of development of a phase, details of the provision of public art within that phase shall be submitted to and approved in writing by the Local Planning Authority. Works shall be carried out in accordance with the approved details prior to the occupation of that phase, and retained as such thereafter.

In the interests of visual amenity and the special character of the listed building.

- 29) Prior to the commencement of development, arrangements for the implementation of highways works to the following areas as identified on approved Sandersons Associates drawing no. TBC shall be submitted to and approved in writing by the Local Planning Authority:
- a. provision of crossing to Hunslet Lane prior to first occupation of phase 3
 - b. provision of bus stop improvements to bus stops 11969 and 11971 on Hunslet Lane including real time information and raised kerbs, prior to first occupation of phase 2
 - c. works including provision of visibility splay onto Hunslet Road of 2.4m x 59m prior to occupation of phase 2
 - d. improvements to Neal Place linking Leathley Road to Butterley Street prior to occupation of phase 3

It is considered that all of the above works should be implemented prior to the relevant phase of the development is occupied and retained as such thereafter, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of pedestrian and vehicular safety.

- 30) The development hereby approved shall not exceed the total amounts of gross floor area / scale of development set out below, without prior approval in writing from the local planning authority after consultation with the Highways Agency:

19,550 sq m of Class B1 employment use with 147 car parking spaces,

(The Use Classes are those set out in the Town and Country Planning (use classes) Order 1987, or in any equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification).

In order to ensure that the level of development can be accommodated within the safe and efficient operation of the highway network.

- 31) The car parking spaces to be provided for the employees and visitors to the Class B1 development hereby approved shall be confined to those areas within the development specifically designated for such purpose.

In order to ensure a satisfactory form of development and to ensure that the number of morning peak arrivals does not exceed the level necessary to ensure safe operation of the highway network.

- 32) The development permitted by this development shall only be carried out in accordance with the approved flood risk assessment (FRA) project no. 1108 revision B and the following mitigation measures detailed within the FRA:
- (a) limiting the existing surface water run-off generated by up to and including the 1 in 100 year storm by 30% so that it will not increase the risk of flooding off-site;
 - (b) flood-proofing measures detailed in section 4.0 of the FRA in the proposed development;
 - (c) finished floor levels are set no lower than 600mm above 25.55m Ordnance Datum (AOD).

In order to prevent flooding by ensuring the satisfactory disposal of surface water from the site, and to reduce the impact of flooding on the proposed development and future occupants.

- 33) No development approved by this permission shall be commenced until a scheme for the provision of an emergency egress and evacuation arrangement has been submitted to and approved in writing by the Local Planning Authority.

To ensure a satisfactory contingency plan for emergency access is provided to the development.

- 34) The landscape details to be submitted in the landscape reserved matters or landscape conditions above shall provide full construction details of tree pits and raised planted areas to be provided over underground basement areas. These shall include:
- a) the depth and area of growing media;
 - b) specification of topsoils including additives and mulches;
 - c) tree grilles and guards and means of anchoring root balls;
 - d) passive irrigation including directed use of grey water / roofwater or surface water;
 - e) active irrigation, using mains water or filtered, re-cycled greywater, its application rate, distribution system and controls;
 - f) means of drainage;
 - g) local modification of the slab and structural column supports to the basement to support the additional loading.
 - h) dimension of tree pits and means of anchoring root balls

To ensure that the cultural requirements for viable landscape and / or tree establishment are integrated into the development scheme design and structure.

- 35) Details and management of the gated accesses onto Hunslet Road and Leathley Road for phase 3 of the proposals shall be submitted and approved in writing by the local planning authority before first construction, and implemented before first occupation of the development, and retained and maintained thereafter.

In the interests of pedestrian and vehicular safety

- 36) The access gates for both the Leathley Road and Hunslet Road for phase 1 and 2 of the proposals must be fully open for all opening times of all buildings on the site, unless other agreed in writing with the local planning authority. The access onto Leathley Road for phases 1 and 2 must be used only as a large vehicle delivery access and must be signed and marked appropriately.

In the interests of pedestrian and vehicular safety

- 37) Prior to the commencement of development of phases 2 and 3, details for the provision of shower facilities/lockers in each building for staff shall be submitted to and approved in writing by the Local Planning Authority. Facilities shall be provided prior to first occupation of each building and retained as such thereafter.

In the interests of the promotion of sustainable forms of travel.

- 3) Unless otherwise agreed in writing by the Local Planning Authority, no works of demolition, or alteration to the interior or exterior of the building, shall take place other than in complete accordance with a programme of demolition and redevelopment identified on Seven Architecture drawing refs. 060127-111 Revision E, 060127-12 Revision E, and 060127-113 Revision D.
- (a) In phase 1, prior to the demolition of Building 2b, details for the temporary treatment of the exposed elevation to Building 2a shall be submitted to and approved in writing by the Local Planning Authority, and implemented upon completion of demolition, unless otherwise agreed in writing.
 - (b) In phase 2, no demolition works to Building 2b shall take place until a contract for the implementation of the new public square and the works to the ground floor treatment of Building 1 shown on Seven Architecture Drawing Ref. 060127-232. These works shall be completed within 3 months of first occupation of the first new office building.
 - (c) Details for each relevant condition below shall then be submitted in accordance with the phasing schedule unless otherwise agreed in writing by the Local Planning Authority. Any subsequent changes to the phasing schedule shall be submitted in writing to and approved by the Local Planning Authority. The scheme shall be built out in accordance with the approved phasing plan.

In order to accord with the provisions of the Leeds Unitary Development Plan Review 2006, in the interests of visual amenity, the special character of the listed building, pedestrian connectivity, highways safety, sustainable development, and in order that the Local Planning Authority is informed of the phasing in order that the relevant sections of the conditions may be discharged.

- 7) No building operations shall take place within a phase until typical 1:20/1:50 details of the following have been submitted to and approved in writing by the Local Planning Authority:
- a) all doorways
 - b) all windows
 - c) eaves and soffit detail
 - d) the external treatment and materials to any roof top plant rooms
 - e) canopy to south eastern gable of print hall

Works shall be carried out in accordance with the details thereby approved, and retained as such thereafter.

In the interests of visual amenity, design quality and the setting of the listed building.

- 17) Prior to the commencement of works for the refurbishment of Building 1, details of the treatment to the columns in relation to the raising of the floor level, shall be submitted to and approved in writing by the Local Planning Authority. Works shall be carried out in accordance with the approved details and retained as such thereafter.

In the interests of the special character and historic interest of the listed building.

- 19) Prior to the commencement of works for the refurbishment of Building 1, details of the restoration works to the print hall balconies, railing and staircases, shall be submitted to and approved in writing by the Local Planning Authority. Works shall be carried out in accordance with the approved details and retained as such thereafter.

In the interests of the special character and historic interest of the listed building.

- 20) The landscape details to be submitted in the landscape reserved matters or landscape conditions above shall provide full construction details of tree pits and raised planted areas to be provided over underground basement areas. These shall include:
- a) the depth and area of growing media;
 - b) specification of topsoils including additives and mulches;
 - c) tree grilles and guards and means of anchoring root balls;
 - d) passive irrigation including directed use of grey water / roofwater or surface water;
 - e) active irrigation, using mains water or filtered, re-cycled greywater, its application rate, distribution system and controls;
 - f) means of drainage;
 - g) local modification of the slab and structural column supports to the basement to support the additional loading.
 - h) dimension of tree pits and means of anchoring root balls

To ensure that the cultural requirements for viable landscape and / or tree establishment are integrated into the development scheme design and structure.

THIS DRAWING TO BE READ IN CONJUNCTION WITH ALL RELEVANT STRUCTURAL AND MECHANICAL + ELECTRICAL ENGINEER DRAWINGS AND SPECIFICATIONS

FOR 'EXISTING HALL / MILL BUILDING' PLANS REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_201-210 INCLUSIVE AND 211-210 FOR 'NEW BUILDING' PLANS.

FOR 'SITE SECTIONS' AS PROPOSED REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_229 AND 230 FOR 'EXISTING BUILDING SECTIONS' AS PROPOSED.

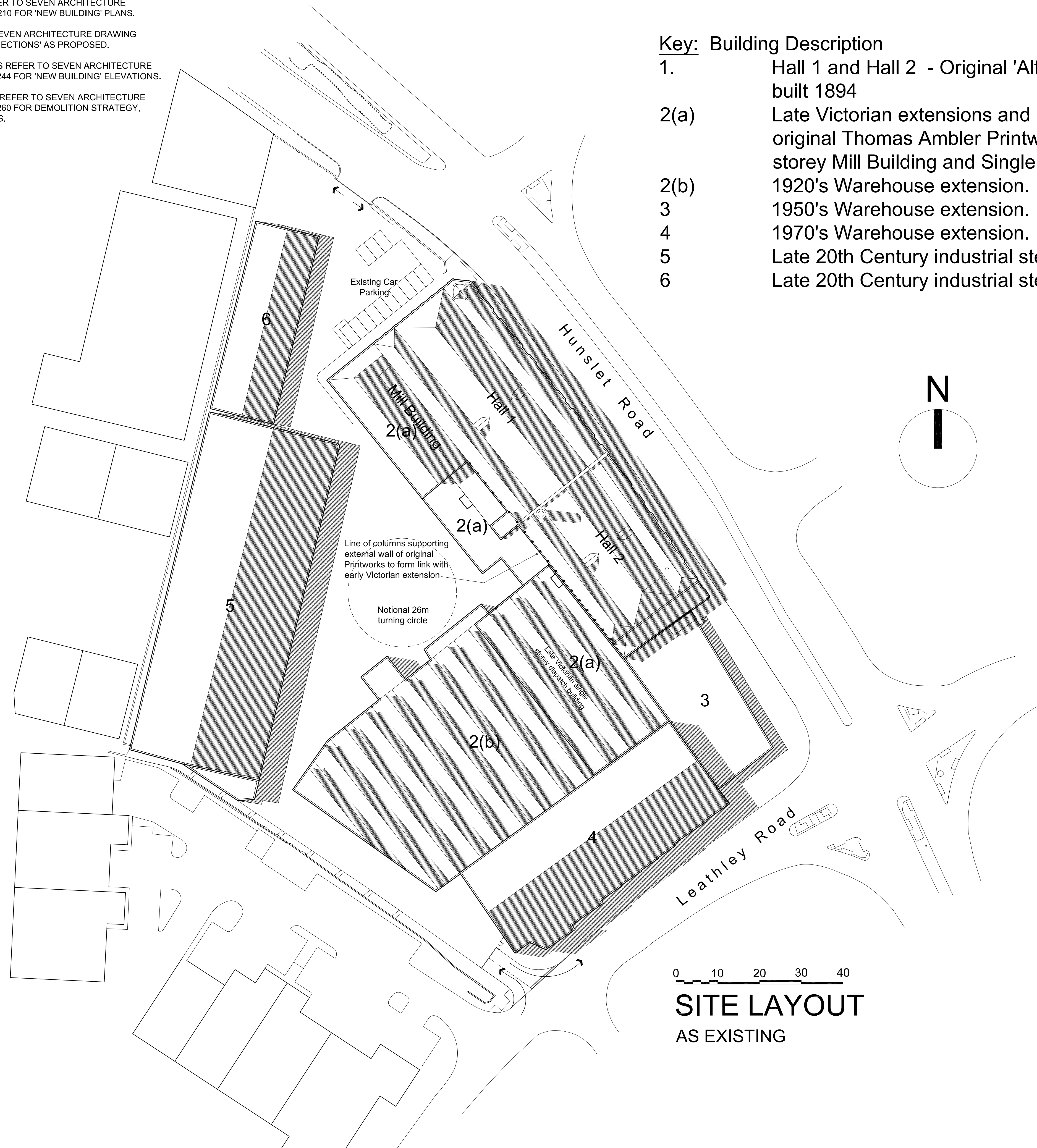
FOR 'EXISTING HALL / MILL BUILDING' ELEVATIONS REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_231-233 INCLUSIVE AND 235-244 FOR 'NEW BUILDING' ELEVATIONS.

FOR 'EXISTING BUILDING' SURVEY INFORMATION REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_261-272 INCLUSIVE AND 251-260 FOR DEMOLITION STRATEGY, PLUS 110-113 FOR SITE PLAN DEMOLITION WORKS.

1. DO NOT SCALE FROM THIS DRAWING - USE FIGURED DIMENSIONS ONLY
2. TO BE READ IN CONJUNCTION WITH ALL OTHER CONSULTANTS' / SPECIALISTS DRAWINGS, REPORT ANY DISCREPANCIES BEFORE AFFECTED WORK COMMENCES
3. ALL SETTING OUT DIMENSIONS TO BE CHECKED ON SITE BY CONTRACTOR BEFORE WORK COMMENCES

Key: Building Description

1. Hall 1 and Hall 2 - Original 'Alf Cooke' Print Works building built 1894
- 2(a) Late Victorian extensions and alterations undertaken to the original Thomas Ambler Printworks Building including the 4 storey Mill Building and Single Storey Dispatch Area
- 2(b) 1920's Warehouse extension.
- 3 1950's Warehouse extension.
- 4 1970's Warehouse extension.
- 5 Late 20th Century industrial steel framed storage building.
- 6 Late 20th Century industrial steel framed storage building.



0 10 20 30 40

**SITE LAYOUT
AS EXISTING**

rev.	description	date	author	checked
A	31/10/07	AMc	MJW	
B	25/01/10	AMc	MJW	
C	07/07/10	AT	MJW	
D	16/11/10	ADC	MJW	

client	The Print Works (Leeds) LLP
project	The Print Works, Leeds
drawing title	Site Layout as Existing
drawing number	060127_100
revision	D
drawing status	Information
scale	1:500@ A1
date	06/03/07
drawn by	CTJ
checked by	MJW

notes

- DO NOT SCALE FROM THIS DRAWING - USE FIGURED DIMENSIONS ONLY
- TO BE READ IN CONJUNCTION WITH ALL OTHER CONSULTANTS' / SPECIALISTS DRAWINGS. REPORT ANY DISCREPANCIES BEFORE AFFECTED WORK COMMENCES
- ALL SETTING OUT DIMENSIONS TO BE CHECKED ON SITE BY CONTRACTOR BEFORE WORK COMMENCES

THIS DRAWING TO BE READ IN CONJUNCTION WITH ALL RELEVANT STRUCTURAL AND MECHANICAL + ELECTRICAL ENGINEER DRAWINGS AND SPECIFICATIONS

FOR 'EXISTING HALL / MILL BUILDING' PLANS REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_201-210 INCLUSIVE AND 211-210 FOR 'NEW BUILDING' PLANS.

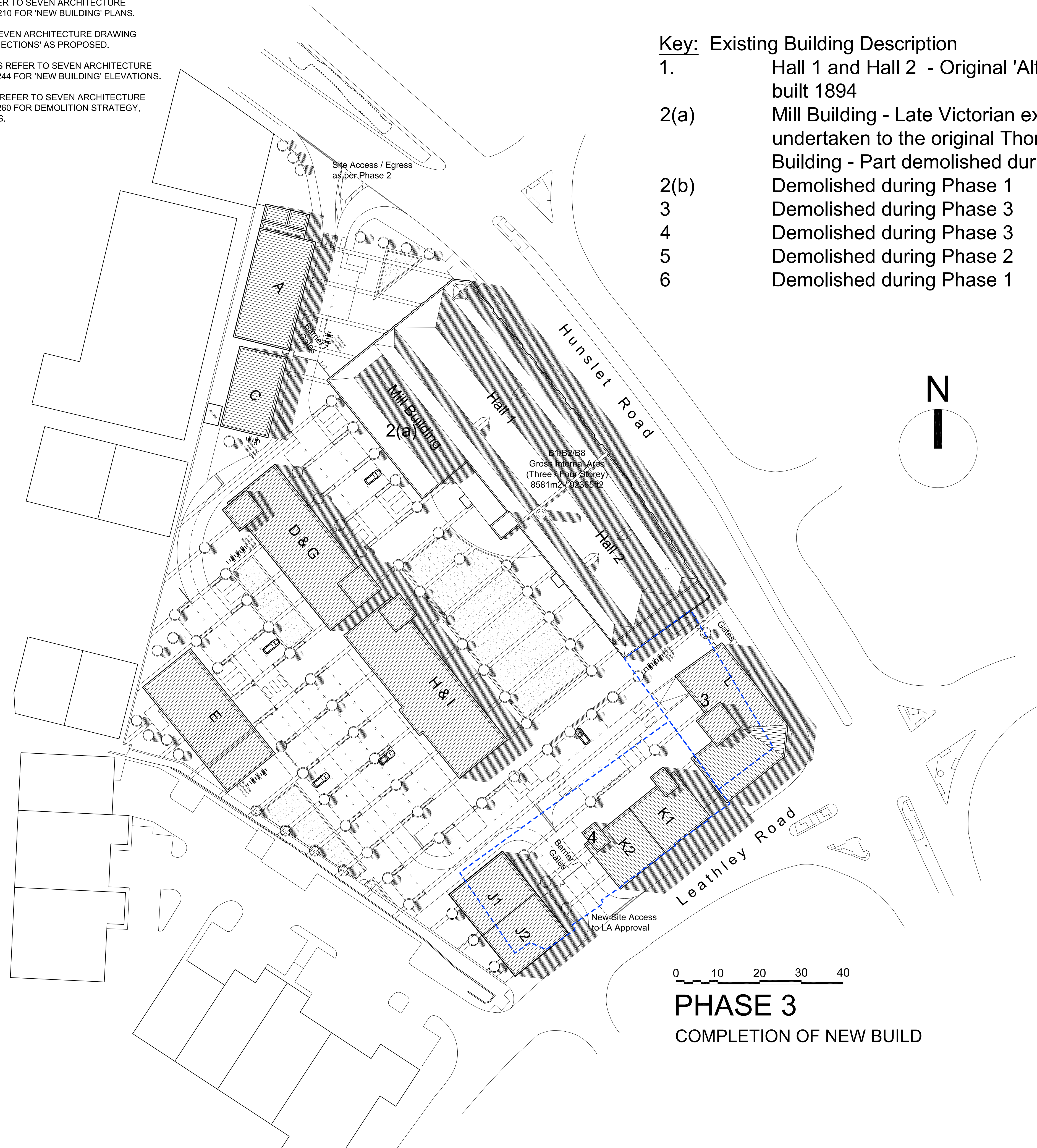
FOR 'SITE SECTIONS' AS PROPOSED REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_229 AND 230 FOR 'EXISTING BUILDING SECTIONS' AS PROPOSED.

FOR 'EXISTING HALL / MILL BUILDING' ELEVATIONS REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_231-233 INCLUSIVE AND 235-244 FOR 'NEW BUILDING' ELEVATIONS.

FOR 'EXISTING BUILDING' SURVEY INFORMATION REFER TO SEVEN ARCHITECTURE DRAWING NO 06127_261-272 INCLUSIVE AND 251-260 FOR DEMOLITION STRATEGY, PLUS 110-113 FOR SITE PLAN DEMOLITION WORKS.

Key: Existing Building Description

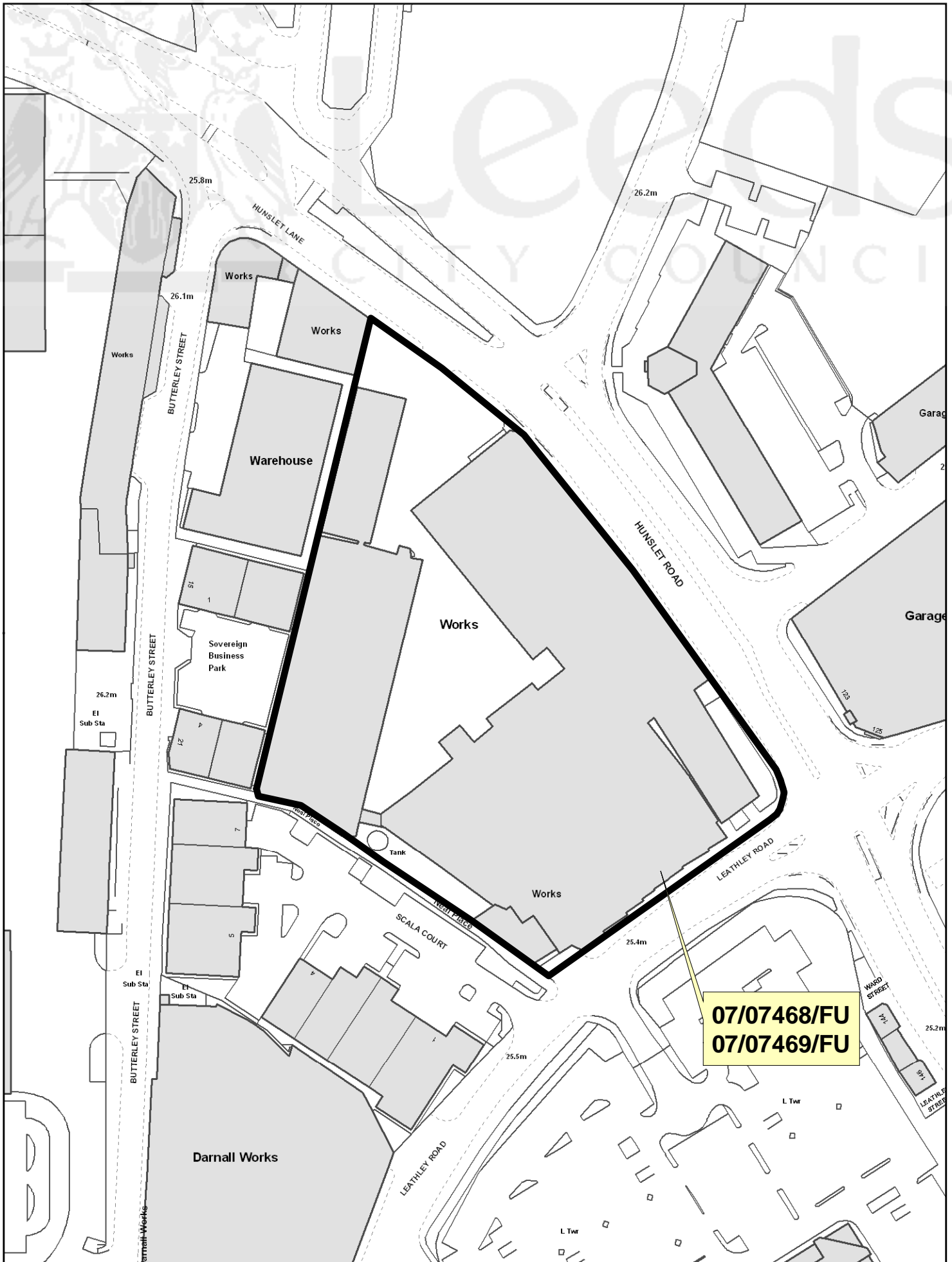
- 1. Hall 1 and Hall 2 - Original 'Alf Cooke' Print Works building built 1894
- 2(a) Mill Building - Late Victorian extensions and alterations undertaken to the original Thomas Ambler Printworks Building - Part demolished during Phase 2
- 2(b) Demolished during Phase 1
- 3 Demolished during Phase 3
- 4 Demolished during Phase 3
- 5 Demolished during Phase 2
- 6 Demolished during Phase 1



rev.	description	date	author	checked
A	Phase 3 demolition works included	06/01/10	ADC	MJW
B	Phase 3 car parking introduced in accordance with masterplan 060127/101G	18/06/10	AT	ADC
C	Further detail added to drawing	07/07/10	AT	MJW
D	Updated in accordance with Phase 2 Dwg	18/11/10	ADC	MJW

client	Rushbond Plc
project	The Print Works, Leeds
drawing title	Phase 3 as Proposed Phasing Strategy
drawing number	060127_113
revision	D
drawing status	Information
scale	1:500@A1
date	10/09/09
drawn by	ADC
checked by	MJW

PHASE 3
COMPLETION OF NEW BUILD



07/07468/FU
07/07469/FU

CITY CENTRE PANEL